

DEKALB COUNTY PUBLIC WORKS

PROPOSED PLAN OF

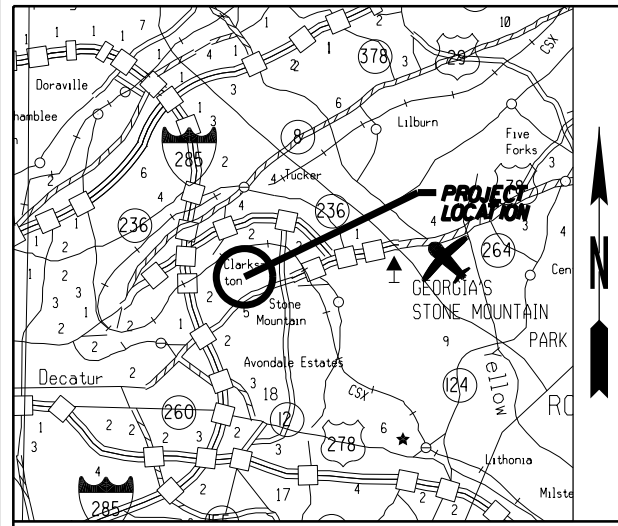
PEDESTRIAN CROSSINGS AT 3 LOCATIONS, CENTRAL DR, INDIAN CREEK WAY, REDAN RD

DEKALB COUNTY

FEDERAL ROUTE • N/A

STATE ROUTE • N/A

P.J.NO. N/A



LOCATION SKETCH
N.T.S.

LOCATION & DESIGN
APPROVAL DATE: N/A

FUNCTIONAL CLASS:
MINOR ARTERIAL

THIS PROJECT IS 100% IN DEKALB
COUNTY AND IS IN CONG. DIST. NO. 6

PROJECT DESIGNATION: EXEMPT

DESIGN DATA:

TRAFFIC A.D.T.: 23800

TRAFFIC D.H.V.: N/A

DIRECTIONAL DIST: N/A

% TRUCKS: N/A

24 HR. TRUCKS %: N/A

SPEED DESIGN: 45 MPH

NOTE :

ALL REFERENCES IN THIS DOCUMENT, WHICH INCLUDES ALL PAPERS, WRITINGS, DOCUMENTS, DRAWINGS, OR PHOTOGRAPHS USED, OR TO BE USED IN CONNECTION WITH THIS DOCUMENT, TO "STATE HIGHWAY DEPARTMENT OF GEORGIA", "STATE HIGHWAY DEPARTMENT", "GEORGIA STATE HIGHWAY DEPARTMENT", "HIGHWAY DEPARTMENT", OR "DEPARTMENT" WHEN THE CONTEXT THEREOF MEANS THE STATE HIGHWAY DEPARTMENT OF GEORGIA, AND SHALL BE DEEMED TO MEAN THE DEPARTMENT OF TRANSPORTATION.

THE DATA, TOGETHER WITH ALL OTHER INFORMATION SHOWN ON THESE PLANS OR IN ANY WAY INDICATED THEREBY, WHETHER BY DRAWINGS OR NOTES, OR IN ANY OTHER MANNER, ARE BASED UPON FIELD INVESTIGATIONS AND ARE BELIEVED TO BE INDICATIVE OF ACTUAL CONDITIONS. HOWEVER, THE SAME ARE SHOWN AS INFORMATION ONLY, ARE NOT GUARANTEED, AND DO NOT BIND THE DEPARTMENT OF TRANSPORTATION IN ANY WAY. THE ATTENTION OF BIDDER IS SPECIFICALLY DIRECTED TO SUBSECTIONS 102.04, 102.05, AND 104.03 OF THE SPECIFICATIONS.



LENGTH OF PROJECT

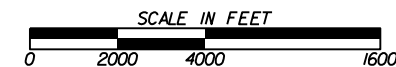
P.I. NO. 0008121/0012789

DeKALB COUNTY
No. 089

MILES

NET LENGTH OF ROADWAY
NET LENGTH OF BRIDGES
NET LENGTH OF PROJECT
NET LENGTH OF EXCEPTIONS
GROSS LENGTH OF PROJECT

N/A

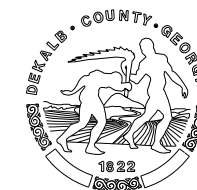


THIS PROJECT HAS BEEN PREPARED
WITHOUT A COORDINATE SYSTEM OR
VERTICAL DATUM.

PLANS PREPARED BY:



UNDER THE SUPERVISION OF:
MATTHEW THOMPSON, P.E.



1/11/24

PLANS COMPLETED 1/11/2024	
REVISIONS	

DRAWING No.

01-0001

GENERAL NOTES

1. THE LOCATIONS OF EXISTING UTILITIES ARE NOT SHOWN ON THE PLANS. UNDERGROUND UTILITY LOCATIONS SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO BEGINNING CONSTRUCTION. IF UTILITIES ARE ENCOUNTERED DURING EXCAVATION OPERATIONS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY FOR INSTRUCTIONS TO RESOLVE ANY CONFLICTS. ANY DAMAGE OR INTERRUPTION OF EXISTING UTILITIES SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR, SHALL BE REPORTED TO THE AFFECTED UTILITY COMPANY AND THE ENGINEER IMMEDIATELY, AND SHALL BE REPAIRED PROMPTLY AT THE CONTRACTOR'S EXPENSE.
2. UTILITY WORK COORDINATION WILL BE REQUIRED AS PART OF THIS CONTRACT. THE CONTRACTOR SHALL BE REQUIRED TO USE THE ONE-CALL CENTER TELEPHONE NUMBER (811) FOR THE PURPOSE OF COORDINATING THE MARKING OF UNDERGROUND UTILITIES.
3. THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN SUFFICIENT BARRICADES, LIGHTS, WARNING SIGNS, AND OTHER TRAFFIC CONTROL METHODS ADJACENT TO EXISTING ROADWAYS, SIDEWALKS AND PARKING AREAS AS MAY BE REQUIRED FOR THE PROTECTION AND SAFETY OF THE PUBLIC. ALL TEMPORARY SIGNING AND PAVEMENT MARKING SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), LATEST EDITION AND/OR AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR TRAFFIC CONTROL.
4. A NOTICE OF INTENT (NOI) WILL NOT BE REQUIRED FOR THIS PROJECT SINCE DISTURBED AREA IS LESS THAN ONE ACRE, PER GEORGIA EPD PERMIT NO. GAR100002. EVEN THOUGH AN NOI IS NOT REQUIRED, THE CONTRACTOR SHALL INSTALL AND MAINTAIN APPROPRIATE EROSION, SEDIMENTATION AND POLLUTION CONTROL BEST MANAGEMENT PRACTICES (BMP) TO PREVENT SEDIMENT FROM LEAVING THE CONSTRUCTION SITES. ALL NON-PAVED DISTURBED AREAS SHALL BE GRASSED, FERTILIZED AND MULCHED IN ACCORDANCE WITH CURRENT GDOT SPECIFICATION SECTION 700-GRASSING. GRASSED AREAS SHALL BE MAINTAINED BY THE CONTRACTOR UNTIL GRASS IS ESTABLISHED AND ACCEPTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR GRADING COMPLETE.
5. NO ADDITIONAL PAYMENT WILL BE MADE FOR REMOVAL OF ITEMS AS CALLED FOR IN THE PLANS OR AS NECESSARY TO CONSTRUCT ELEMENTS OF THE PROJECT. THE COST FOR ALL REMOVALS SHALL BE INCLUDED IN THE PRICE BID FOR GRADING COMPLETE.
6. ALL INCIDENTAL SAW CUTTING COSTS SHALL BE INCLUDED IN THE PRICE BID FOR GRADING COMPLETE.
7. DEMOLITION DEBRIS SHALL BECOME PROPERTY OF THE CONTRACTOR AND WASTE SOILS, VEGETATION, AND OTHER DELETERIOUS MATERIALS SHALL BE HAULED OFF-SITE AND BE DISPOSED OF AT AN APPROVED LOCATION IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS PER GDOT SPECIFICATION SECTION 201. NO ADDITIONAL PAYMENT WILL BE MADE FOR THIS REQUIREMENT. REMOVAL AND DISPOSAL COSTS SHALL BE INCLUDED IN THE PRICE BID FOR GRADING COMPLETE.
8. ALL PAVEMENT MARKING SHALL BE THERMOPLASTIC AND SHALL BE MANUFACTURED AND INSTALLED IN ACCORDANCE WITH CURRENT GDOT SPECIFICATION SECTION 653-THERMOPLASTIC TRAFFIC STRIPE AND SECTION 657-PREFORMED PLASTIC PAVEMENT MARKINGS.
9. ALL STANDARD HIGHWAY SIGNS SHALL BE FABRICATED AND ERECTED IN ACCORDANCE WITH THE DETAILS SHOWN IN THE PLANS, THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), CURRENT EDITION, AND THE GEORGIA SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS, AND/OR SPECIAL PROVISIONS.
10. ALL STANDARD HIGHWAY SIGNS SHALL BE ERECTED AT A HEIGHT OF 7 FEET ABOVE THE NORMAL EDGE OF PAVEMENT, CURB OR SIDEWALK TO THE BOTTOM OF THE SIGN OR ASSEMBLY.
11. THE HORIZONTAL CLEARANCE FOR STANDARD HIGHWAY SIGNS IN NON-MOUNTABLE CURB SECTIONS SHALL BE AT LEAST 2 FEET FROM THE CURB FACE TO THE NEARER EDGE OF THE SIGN(S).
12. TYPE 11 (VERY HIGH INTENSITY) FLUORESCENT YELLOW-GREEN REFLECTIVE SHEETING SHALL BE USED FOR ALL PEDESTRIAN AND SCHOOL WARNING SIGNS.
13. ALL CURB CUT RAMPS AND SIDEWALKS WITHIN THE TURNING RADII OF ROADS OR DRIVEWAYS SHALL BE 8-INCH CONCRETE. ALL COSTS ASSOCIATED WITH THIS REQUIREMENT SHALL BE INCLUDED IN THE PRICE BID FOR CONC SIDEWALK, 8IN.
14. ALL CONCRETE TO BE USED FOR THE CONSTRUCTION OF RAMPS, SIDEWALKS AND CURB AND GUTTER TO BE CLASS 'A' CONCRETE. CONCRETE SHALL BE PREPARED AND INSTALLED IN ACCORDANCE WITH CURRENT GDOT SPECIFICATION SECTION 441-MISCELLANEOUS CONCRETE.
15. ALL EXISTING ELECTRICAL BOXES, WATER METER BOXES, VALVE BOXES AND ANY OTHER UTILITY STRUCTURES WHICH ARE TO REMAIN SHALL BE SET FLUSH WITH THE TOP OF THE PROPOSED GRADE OR CONCRETE SURFACE. ALL COSTS ASSOCIATED WITH THIS REQUIREMENT SHALL BE INCLUDED IN THE PRICE BID FOR GRADING COMPLETE.
16. CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE ACROSS DISTURBED AREAS AND INTO DRAINAGE FEATURES.
17. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN SUITABLE BORROW MATERIAL AS NEEDED TO COMPLETE THE PROJECT AT NO ADDITIONAL COST. ALL COSTS ASSOCIATED WITH THIS REQUIREMENT SHALL BE INCLUDED IN THE PRICE BID FOR GRADING COMPLETE.
18. EQUIPMENT AND MATERIALS SHALL BE STORED OFF-SITE OR IN AREAS APPROVED BY THE ENGINEER. CONSTRUCTION AND STORAGE AREAS SHALL BE KEPT NEAT AND CLEAN. EQUIPMENT AND MATERIAL SHALL NOT BE STORED WITHIN THE DRIP LINE OF TREES.
19. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES BETWEEN PLAN AND FIELD CONDITIONS PROMPTLY UPON DISCOVERY.
20. RECTANGULAR RAPID FLASHING BEACON (RRFB) ASSEMBLIES AND ACCESSORIES SHALL BE SUPPLIED FROM SOURCES THAT ARE ON GDOT'S QUALIFIED PRODUCTS LIST (QPL) AND SHALL BE MANUFACTURED AND INSTALLED IN ACCORDANCE WITH CURRENT FHWA GUIDANCE. EACH RRFB ASSEMBLY, INCLUDING POST, FOUNDATIONS, POWER SOURCE, PUSH BUTTON, COMMUNICATIONS AND OTHER INCIDENTAL ITEMS TO PROVIDE A COMPLETE AND FUNCTIONING ASSEMBLY, SHALL BE INCLUDED IN LUMP SUM PRICE BID FOR THE COMPLETION OF THE PROJECT. SIGNS MOUNTED ON THE RRFB ASSEMBLY SHALL BE INCLUDED IN LUMP SUM PRICE BID FOR THE COMPLETION OF THE PROJECT. RRFB ASSEMBLY COMPONENT AND INSTALLATION SPECIFICATIONS INCLUDE:
 - a) RRFB ASSEMBLIES SHALL BE MOUNTED ON 4-1/2-INCH STEEL OR ALUMINUM POLES WITH BLACK FINISH. INCLUDE A MATCHING PEDESTAL BASE SET ON A CONCRETE FOUNDATION AND ANCHORED BY FOUR 3/4-INCH BY 18-INCH-LONG ANCHOR BOLTS.
 - b) RRFB SHALL BE BULLDOG PUSH BUTTON ACTIVATED WITH VISUAL LED AND TWO-TONE AUDIBLE CONFIRMATION. SUPPLEMENTAL RRFB ASSEMBLIES SHALL NOT INCLUDE PUSH BUTTONS.
 - c) BEACONS SHALL BE ACTIVATED THROUGH ENCRYPTED, WIRELESS COMMUNICATIONS. MULTIPLE CHANNELS SHALL BE PROVIDED SO RRFB ASSEMBLIES WITHIN A WARNING GROUP WILL FUNCTION SIMULTANEOUSLY.
 - d) LIGHT BARS SHALL BE BLACK.
 - e) FOR SOLAR POWERED INSTALLATIONS, SOLAR PANELS SHALL BE INSTALLED TO OPTIMIZE SUNLIGHT ON THE PANEL. SOLAR PACKAGES SHALL INCLUDE A 13-WATT MINIMUM SOLAR PANEL AND A 12-VOLT BATTERY WITH MINIMUM 14 AMP-HOUR CAPACITY. HIGH CAPACITY SOLAR PACKAGES SHALL INCLUDE A 30-WATT MINIMUM SOLAR PANEL AND A 12-VOLT BATTERY WITH MINIMUM 35 AMP-HOUR CAPACITY.
 - f) RRFB ASSEMBLIES SHALL BE INSTALLED PER MANUFACTURER'S SPECIFICATIONS.
 - g) HIGHWAY SIGNS ATTACHED TO THE RRFB ASSEMBLIES AS CALLED FOR ON THE PLANS SHALL BE INCLUDED IN LUMP SUM PRICE BID FOR THE COMPLETION OF THE PROJECT.
 - h) SHOP DRAWINGS OF EACH RRFB ASSEMBLY TYPE SHALL BE PROVIDED FOR REVIEW AND APPROVAL BY THE ENGINEER.
 - i) THE DURATION OF THE OPERATION OF EACH RRFB FOLLOWING EACH ACTUATION SHOULD BE BASED ON THE PROCEDURES PROVIDED IN SECTION 4E.06 OF THE CURRENT EDITION OF THE MUTCD FOR THE TIMING OF PEDESTRIAN CLEARANCE TIMES FOR PEDESTRIAN SIGNALS.
 - j) EACH RRFB ASSEMBLY SHALL BE TESTED AND VERIFIED BY THE ENGINEER AFTER INSTALLATION TO ENSURE OPERATIONS ARE IN ACCORDANCE WITH THE MANUFACTURER'S INSTALLATION INSTRUCTIONS AND IN ACCORDANCE WITH THE CURRENT FHWA GUIDANCE.
 - k) ALL ELEMENTS OF THE RRFB ASSEMBLY DESCRIBED ABOVE, ALONG WITH THEIR INSTALLATION, SHALL BE INCLUDED IN LUMP SUM PRICE BID FOR THE COMPLETION OF THE PROJECT.

REVISION
 NO. 1
 DATE: 10/29/2019
 BY: [Signature]

REVISION
 NO. 2
 DATE: 10/29/2019
 BY: [Signature]



REVISION DATES		GENERAL NOTES	
		PEDESTRIAN CROSSING IMPROVEMENTS CENTRAL DR, INDIAN CREEK WAY, REDAN RD	
CHECKED:	DATE:	DRAWING No.	
BACKCHECKED:	DATE:	04-0001	
CORRECTED:	DATE:		
VERIFIED:	DATE:		

SUMMARY OF QUANTITIES

**TRAFFIC CONTROL -
PROJECT PI 000000**
LUMP SUM

CURB & GUTTER, MEDIAN AND SIDEWALK		
LOCATION	CONC SIDEWALK, 4 IN X 30 IN, SY	CONC CURB & GUTTER, 8 IN X 30 IN, TP 2 LF
CENTRAL DRIVE	17	42
INDIAN CREEK WAY	100	30
REDAN ROAD	21	39
AS DIRECTED BY ENGINEER:		
PROJECT TOTAL	138	111

MARKING	
LOCATION	THERMOPLASTIC PAVEMENT MARKINGS SOLID TRAFFIC STRIPE 8 INCH WHITE LF
CENTRAL DRIVE	
INDIAN CREEK WAY	111
REDAN ROAD	110
AS DIRECTED BY ENGINEER:	
PROJECT TOTAL	221

DRAINAGE			
STR NO.	LOCATION	STORM DRAIN PIPE	FLARED END SECTION STD 1120
		18 INCH H=1-10 LF	18 INCH STORM DRAIN EA
1	REDAN ROAD	15.0	1
PROJECT TOTAL		15	1

**GRADING COMPLETE -
PROJECT**
LUMP SUM

*GRADING COMPLETE SHALL ALSO INCLUDE REMOVAL OF ITEMS AS CALLED FOR IN THE PLANS OR AS NECESSARY TO CONSTRUCT ELEMENTS OF THE PROJECT; ALL INCIDENTAL SAW CUTTING COSTS; REMOVAL AND DISPOSAL OF DEMOLITION DEBRIS, WASTE SOILS, VEGETATION AND OTHER DELETERIOUS MATERIAL; SETTING UTILITY STRUCTURES THAT ARE TO REMAIN FLUSH WITH PROPOSED GRADE OR CONCRETE SURFACE; EROSION, SEDIMENTATION AND POLLUTION CONTROL BEST MANAGEMENT PRACTICES; GRASSING AND MULCH; AND OBTAINING SUITABLE BORROW MATERIAL.

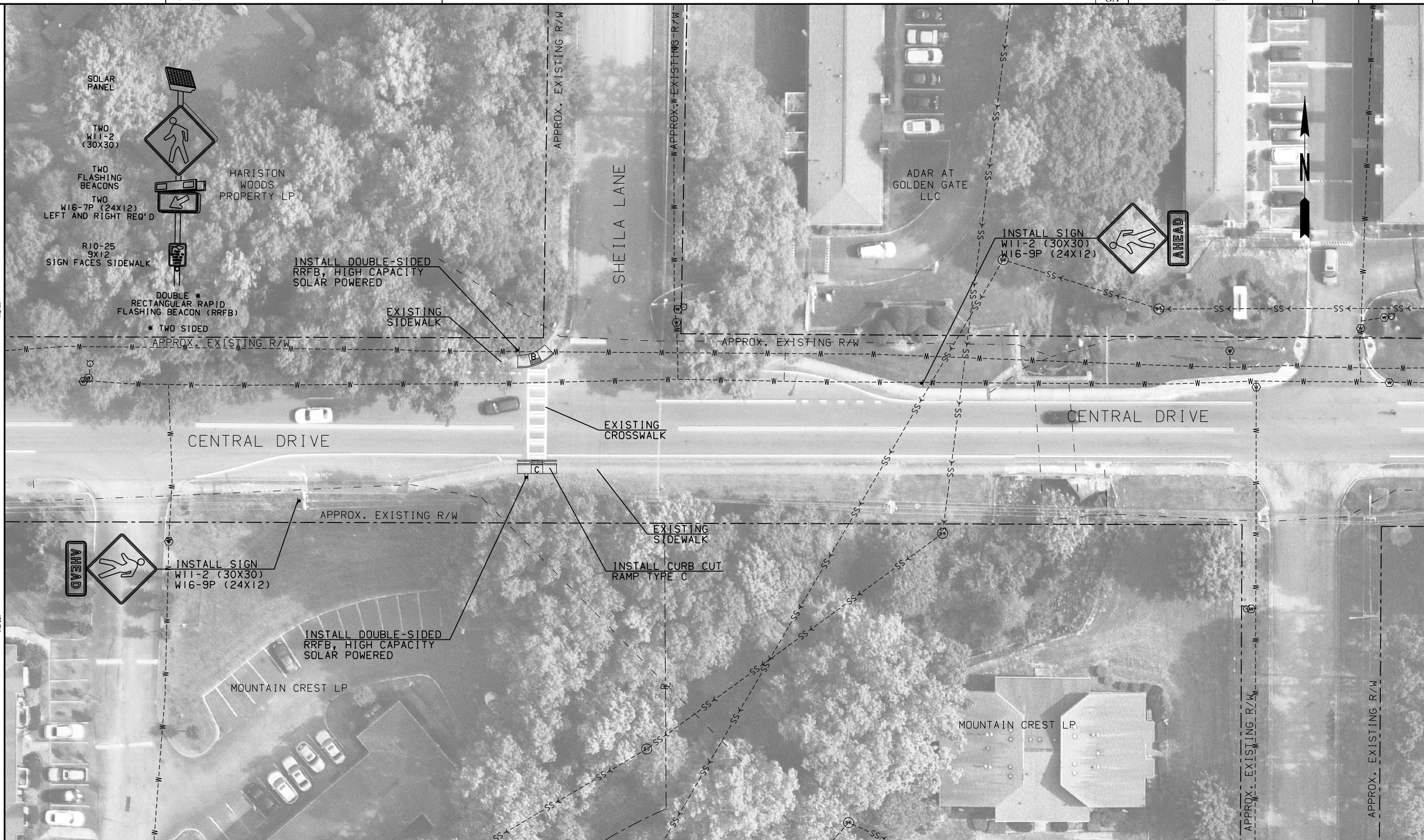
RECTANGULAR RAPID FLASHING BEACONS (RRFB)	
LOCATION	RECTANGULAR RAPID FLASHING BEACON ASSEMBLY DOUBLE SIDED RRFB, HIGH CAPACITY SOLAR POWERED EA
CENTRAL DRIVE	2
INDIAN CREEK WAY	2
REDAN ROAD	2
AS DIRECTED BY ENGINEER:	
PROJECT TOTAL	6

PERMANENT EROSION CONTROL	
LOCATION	STN DUMPED RIP RAP, TP 3, 18 IN SY
AS DIRECTED BY ENGINEER:	
PROJECT TOTAL	8

SIGNING											
LOCATION	MUTCD SIGN CODE	HIGHWAY SIGNS			POSTS						
		REFLECTIVE SHEETING TYPE 11			TYPE 7		TYPE 9				
		TYPE 1 MATERIAL	LENGTH LF	QTY	SF	QTY	TOTAL LF	LENGTH LF	QTY	TOTAL LF	
CENTRAL DRIVE	W11-2	30X30	2	12.50					15.00	2	30.00
	W16-9P	24X12	2	4.00							
INDIAN CREEK WAY	S1-1	36X36	2	13.50	15.00	2	30.00				
	W16-9P	24X12	2	4.00							
REDAN ROAD	W11-2	30X30	2	12.50					15.00	2	30.00
	W16-9P	24X12	2	4.00							
AS DIRECTED BY ENGINEER:											
PROJECT TOTAL				51			30			60	



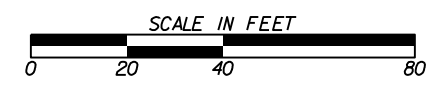
REVISION DATES		SUMMARY QUANTITIES	
PEDESTRIAN CROSSING IMPROVEMENTS		CENTRAL DR, INDIAN CREEK WAY, REDAN RD	
CHECKED:	DATE:	DRAWING No.	
BACKCHECKED:	DATE:	06-0001	
CORRECTED:	DATE:		
VERIFIED:	DATE:		



SEE PLAN SHEET 13-0001 FOR RRFB LOCATIONS

SEE PLAN SHEET 13-0001 FOR RRFB LOCATIONS

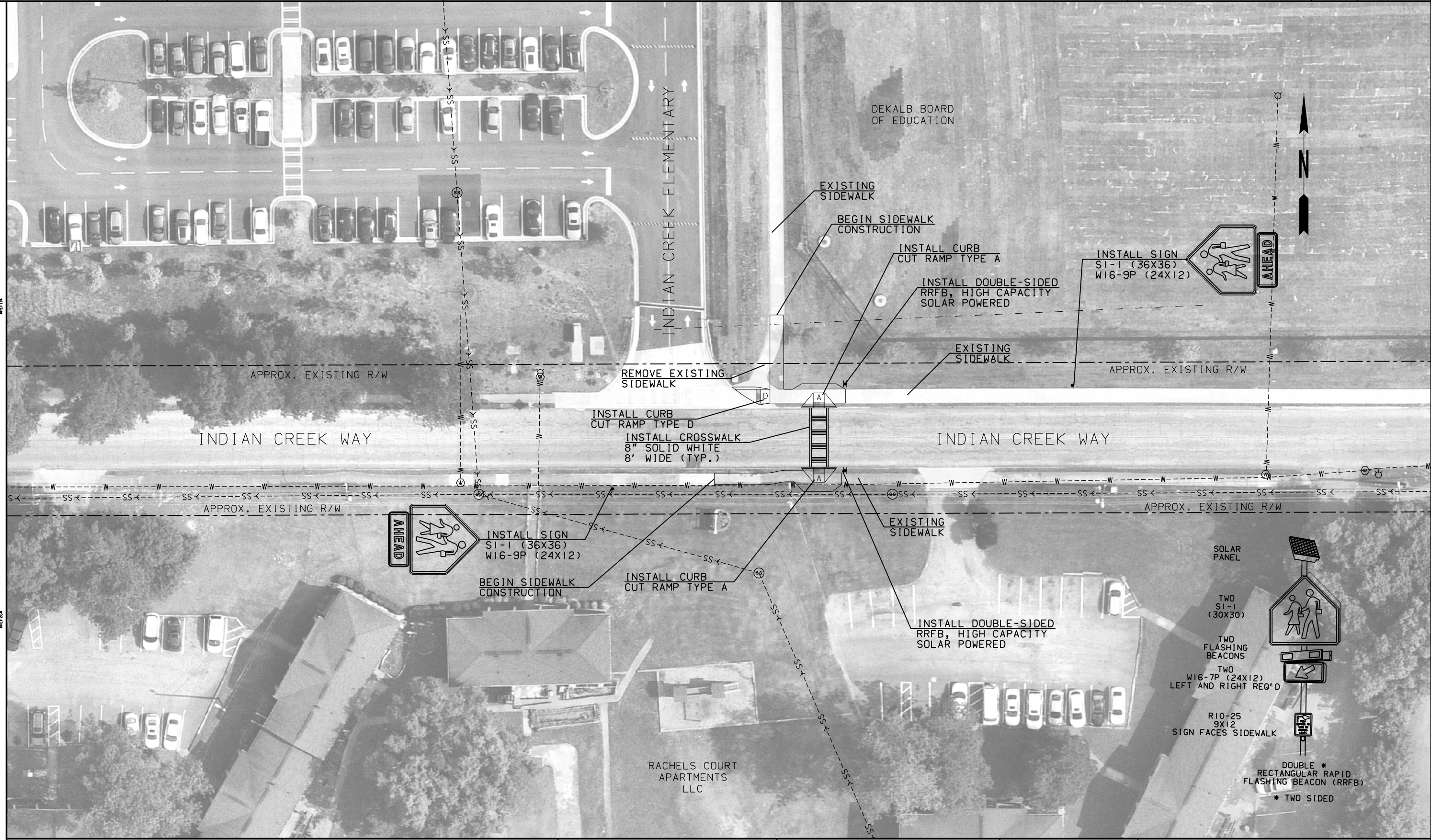
10/29/2019 CONBOR



REVISION DATES	

CONSTRUCTION PLAN
PEDESTRIAN CROSSING IMPROVEMENTS
CENTRAL DR, INDIAN CREEK WAY, REDAN RD

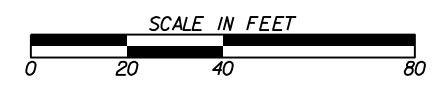
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VERIFIED:	DATE:	



REF: 10/29/2019

REF: 10/29/2019

10/29/2019 CONBOR



REVISION DATES

CONSTRUCTION PLAN
PEDESTRIAN CROSSING IMPROVEMENTS
CENTRAL DR, INDIAN CREEK WAY, REDAN RD

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	13-0002
CORRECTED:	DATE:	
VERIFIED:	DATE:	

DOUBLE *
RECTANGULAR RAPID
FLASHING BEACON (RRFB)
* TWO SIDED

R10-25
9X12
SIGN FACES SIDEWALK

TWO
W16-7P (24X12)
LEFT AND RIGHT REQ'D

TWO
FLASHING
BEACONS

TWO
S1-1
(30X30)

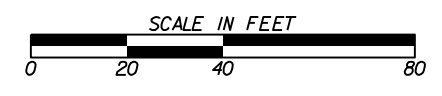
SOLAR
PANEL



SEE PLAN SHEET 13-0003 FOR DETAILS

SEE PLAN SHEET 13-0003 FOR DETAILS

SEE PLAN SHEET 13-0003 FOR DETAILS



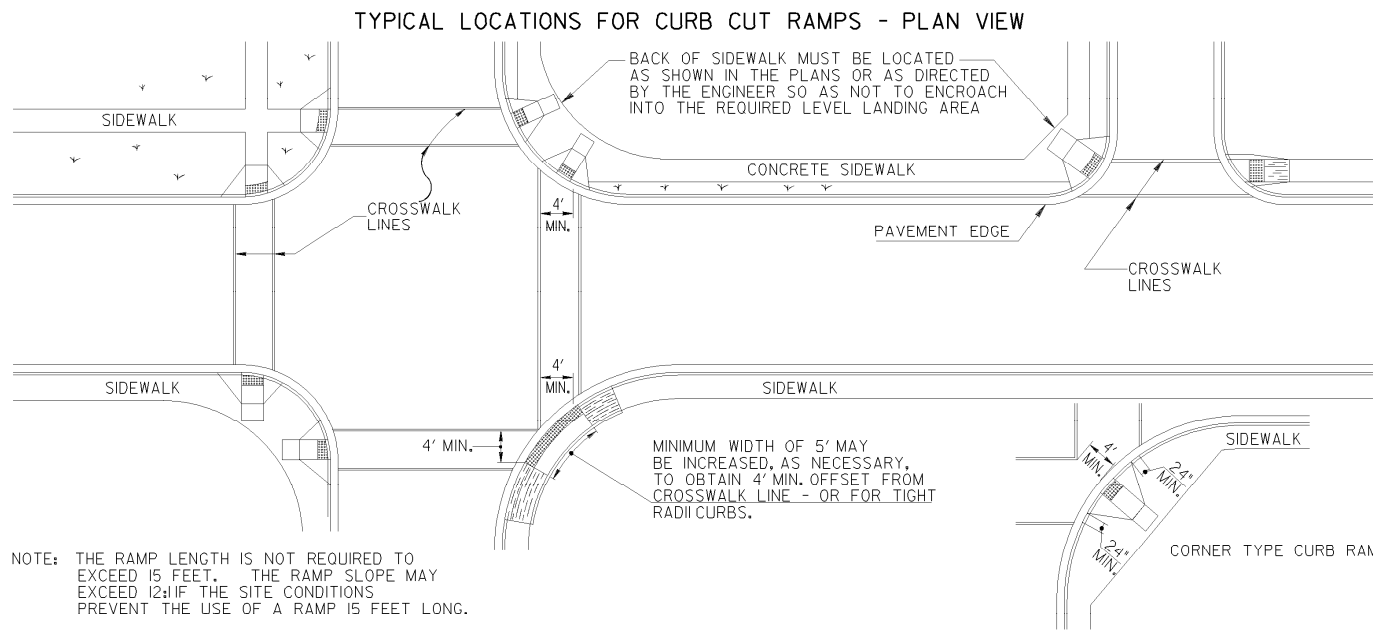
REVISION DATES

CONSTRUCTION PLAN

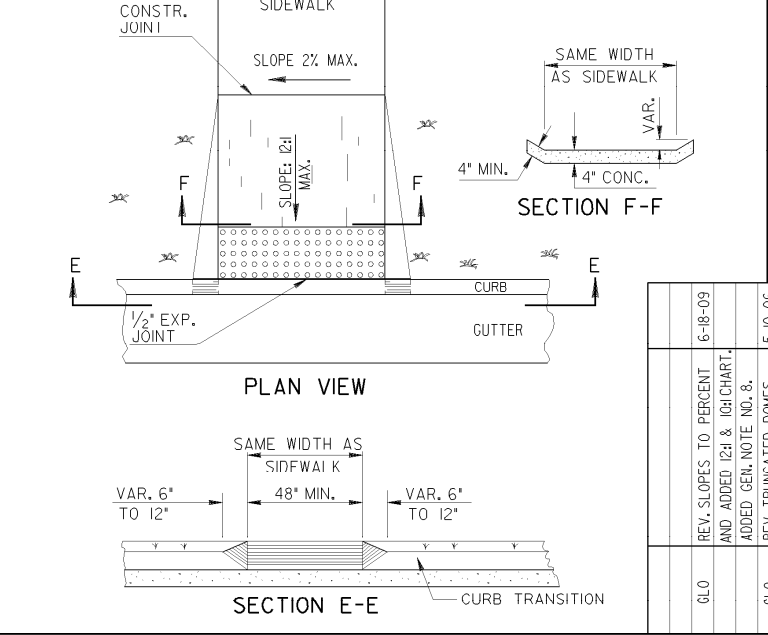
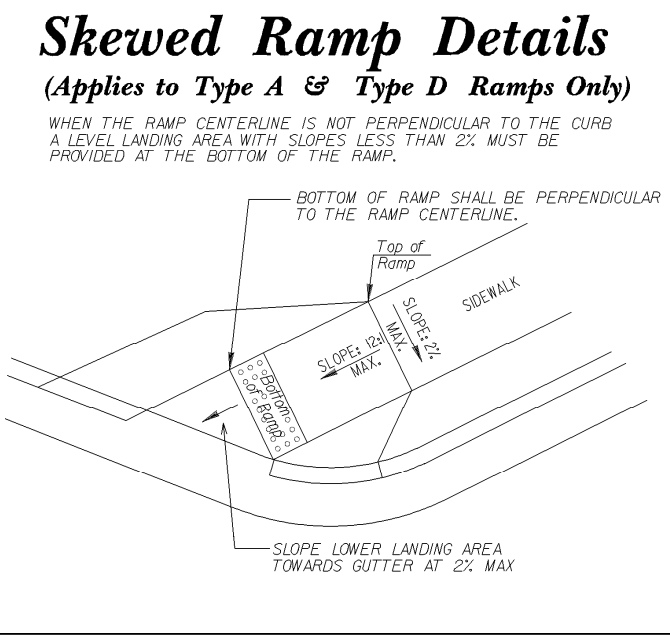
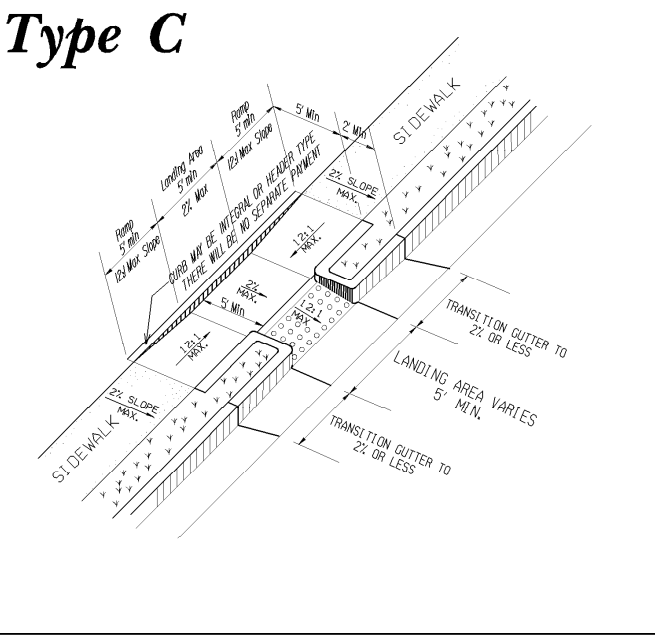
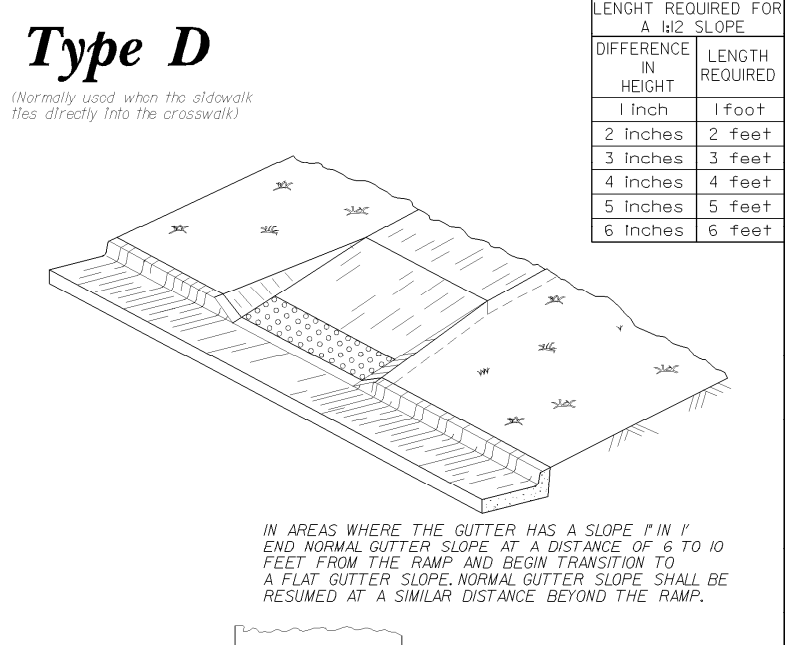
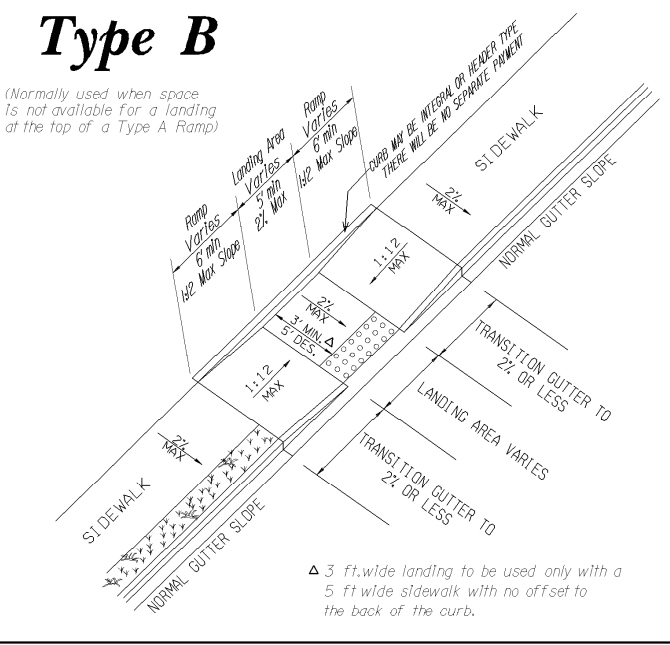
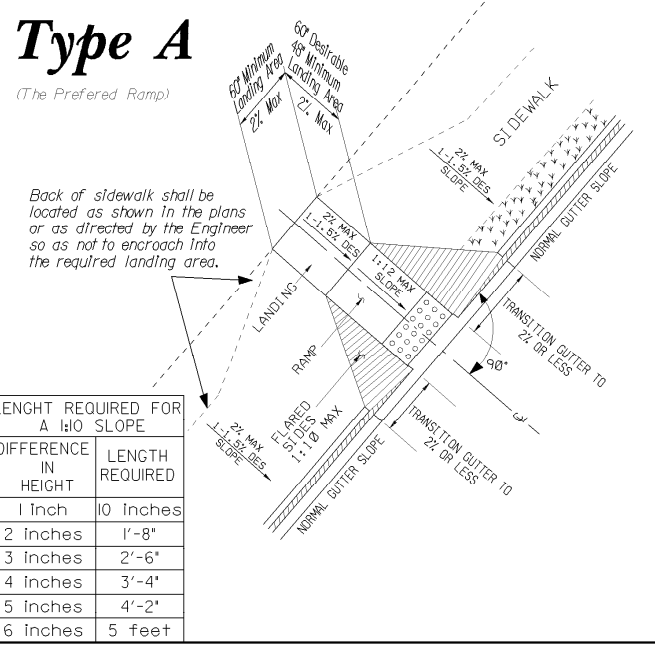
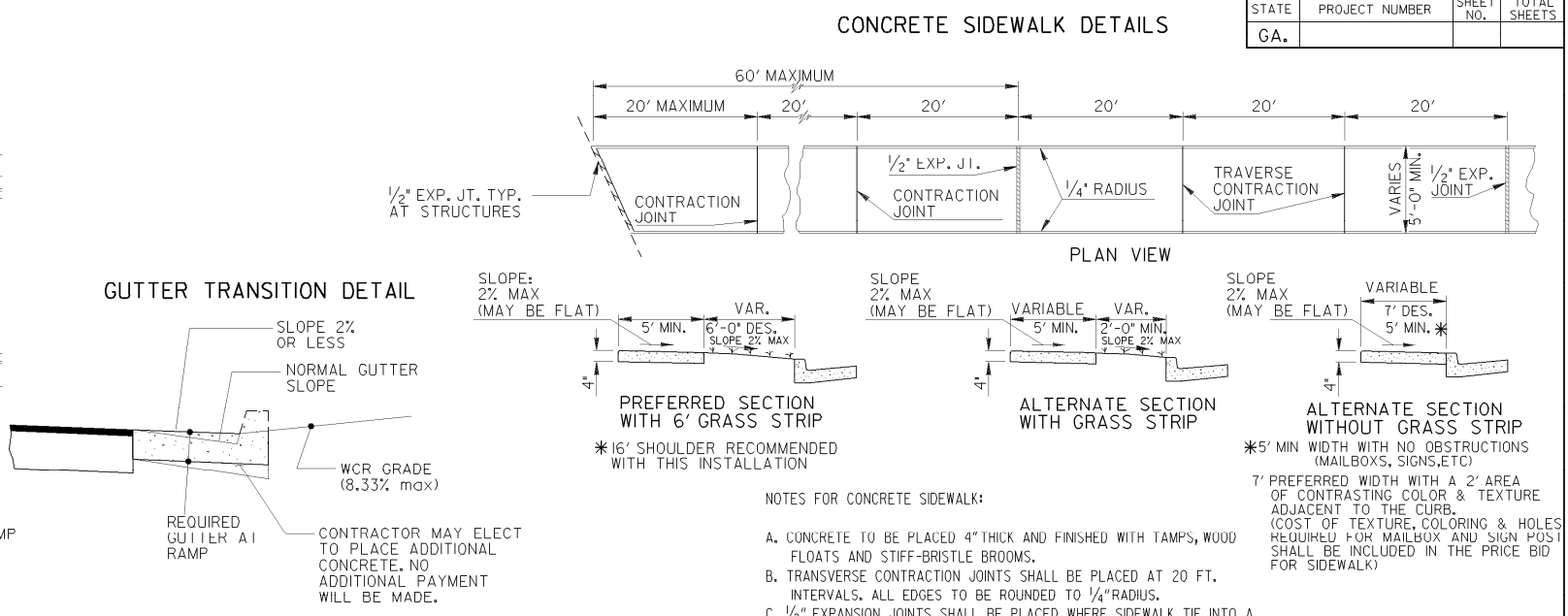
PEDESTRIAN CROSSING IMPROVEMENTS
CENTRAL DR, INDIAN CREEK WAY, REDAN RD

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	13-0003
CORRECTED:	DATE:	
VERIFIED:	DATE:	

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			



NOTE: THE RAMP LENGTH IS NOT REQUIRED TO EXCEED 15 FEET. THE RAMP SLOPE MAY EXCEED 12:1 IF THE SITE CONDITIONS PREVENT THE USE OF A RAMP 15 FEET LONG.



NOTES FOR CONCRETE SIDEWALK:

- CONCRETE TO BE PLACED 4\"/>

NOTES FOR CURB CUT RAMPS:

- CURB CUT RAMPS WILL BE LOCATED AS FOLLOWS UNLESS PLANS OR CONTRACT SPECIFY OTHERWISE.
 - AT ALL PEDESTRIAN CROSSWALKS WHERE CURB IS CONSTRUCTED OR REPLACED.
 - WHERE THE SIDEWALK, CONCRETE OR UNPAVED, IS INTERRUPTED BY THE CURB AT TURNOUTS OR AT INTERSECTIONS.
 - AT OTHER LOCATIONS SUCH AS HOSPITALS, NURSING HOMES, REST AREAS, ETC., WHERE THE CURB WOULD OTHERWISE BE AN OBSTRUCTION TO THE PHYSICALLY DISABLED.

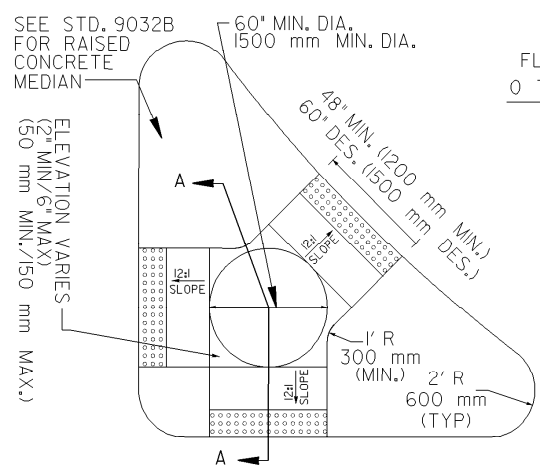
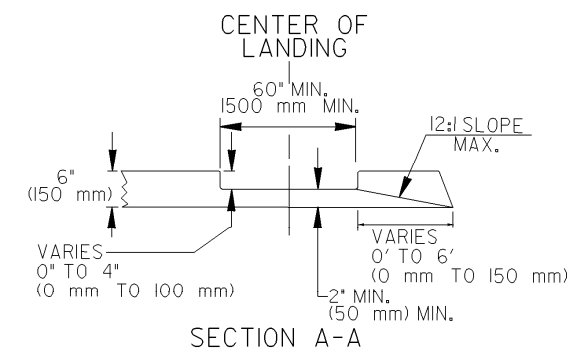
- RAMPS WILL BE CONSTRUCTED FROM CONCRETE. SPECIFICATIONS FOR RAMPS WILL BE THE SAME AS FOR CONCRETE SIDEWALK. RAMPS SHALL HAVE EITHER A ROUGH OR A TEXTURED FINISH.
- DROP INLETS ARE NOT TO BE LOCATED DIRECTLY IN FRONT OF RAMPS. CATCH BASINS SHOULD BE LOCATED AT LEAST 10 FT. FROM RAMPS WHEN FEASIBLE.
- WHERE RAMPS ARE LOCATED IN RADII, THE DIMENSIONS SHOWN FOR RAMP WIDTHS AND TAPERS ARE MEASURED PERPENDICULAR TO THE RAMP AND NOT ALONG THE CURVE.
- WHERE UTILITY STRUCTURES CONFLICT, WHERE SIDEWALK GEOMETRY VARIES, AT SKEWED INTERSECTIONS, OR IN OTHER SPECIAL CASES, THE RAMP DESIGNS MAY BE MODIFIED BY THE DESIGNER OR ENGINEER, PROVIDED THAT THE WIDTH REMAINS A MINIMUM OF 48 INCHES, AND NO SLOPE ON THE ACCESSIBLE PART OF THE RAMP IS STEEPER THAN 12:1.
- 10 FT. OF CURB AND GUTTER WILL INCLUDE THE TRANSITIONED CURB IN FRONT OF RAMPS. 50 YDS. OF CONCRETE SIDEWALK AND CONCRETE MEDIAN PAVING WILL INCLUDE RAMPS. NO ADDITIONAL PAYMENT WILL BE MADE FOR CURB RAMPS. NO ADDITIONAL PAYMENT WILL BE MADE FOR SAWING AND REMOVING EXISTING SIDEWALK OR CURB WHERE NECESSARY FOR RAMP CONSTRUCTION.
- WHEN A CURB RAMP IS PLACED ON EXISTING PAVEMENT, THE PAVEMENT SHALL BE REMOVED TO PROVIDE A MINIMUM THICKNESS OF 3 INCHES OF CONCRETE AT ALL LOCATIONS. NO SEPARATE PAYMENT WILL BE MADE FOR REMOVAL OF THE PAVEMENT.
- DETECTABLE WARNING SURFACES ARE REQUIRED ON ALL INTERSECTIONS WITH PUBLIC STREETS, SIGNALIZED COMMERCIAL DRIVEWAYS, AND COMMERCIAL DRIVEWAYS WITH AN AADT OF 25 VPD.

This Detail Replaces Ga Standard 9031W
Guidelines For Usage On Metric Projects

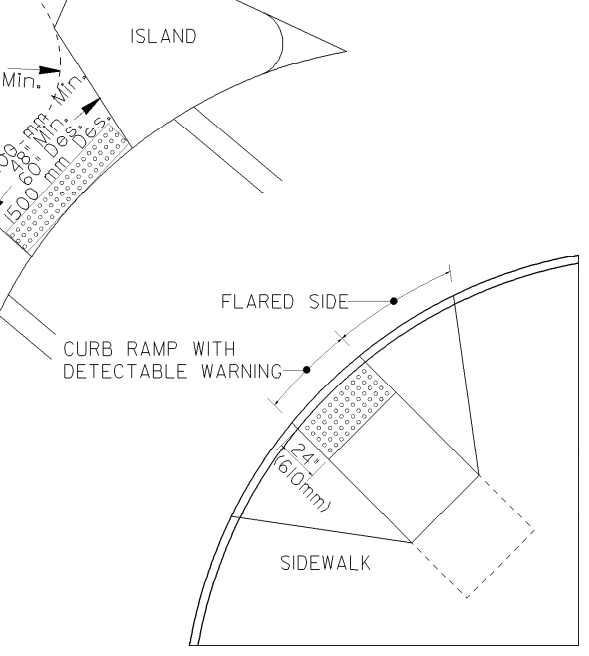
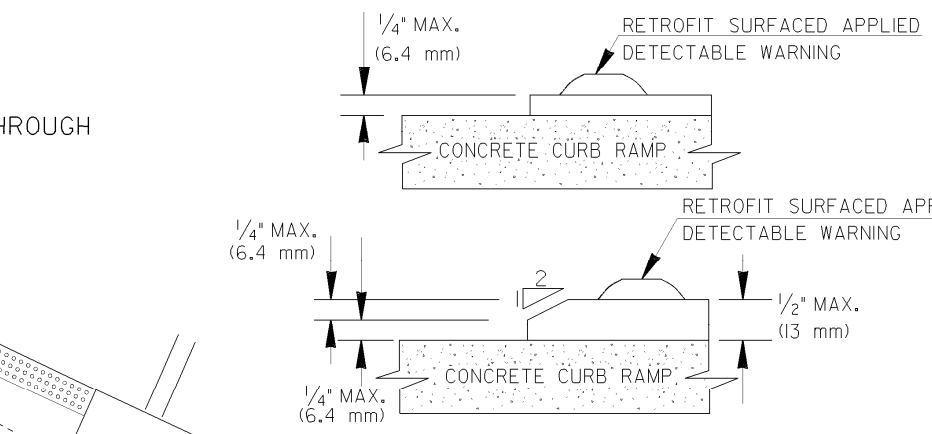
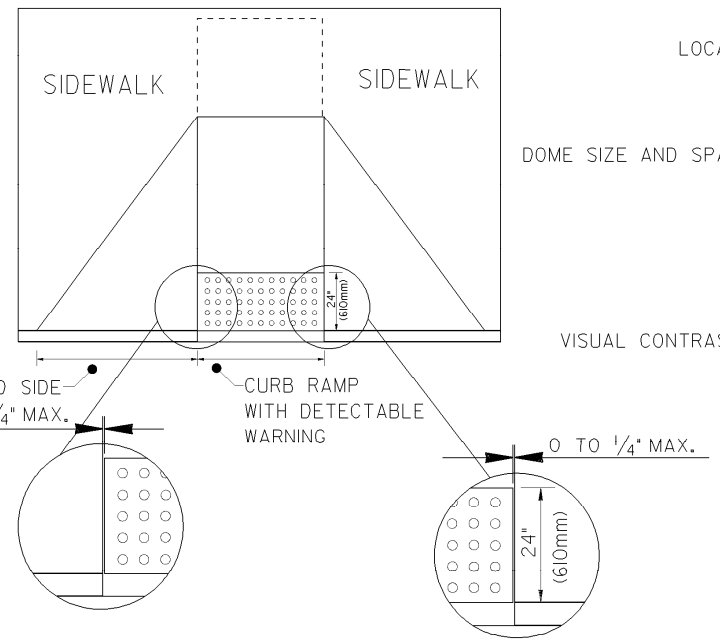
When these details are incorporated into plans and or projects that are being prepared or constructed in metric units, exact or precise conversion to metric units is not required. The dimensions shown that are in feet and inches may be converted to corresponding metric units using the following "Rounded-Off" conversion factors: 1" = 25mm, 4" = 100mm, and 12" or 1' = 300mm. All measurement notes that refer to linear feet and square yards shall be interpreted to mean linear meters and square meters.

REV. SLOPES TO PERCENT AND ADDED 12:1 & 10:1 CHART. ADDED GEN. NOTE NO. 8.	6-18-09	DATE	DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA
REV. TRUNCATED DOMES REVISION	5-10-06	DATE	SPECIAL DETAIL CONCRETE SIDEWALK DETAILS CURB CUT (WHEELCHAIR) RAMPS
REVISION	2-21-03	DATE	
REVISION	2-10-03	DATE	
REVISION	7-29-02	DATE	
REVISION	5-29-02	DATE	
REVISION	5-23-02	DATE	
REVISION	5-13-02	DATE	
REVISION	4-29-02	DATE	NO SCALE
REVISION	4-11-02	DATE	MARCH 12, 2002
REVISION	4-3-02	DATE	NUMBER
REVISION	3-28-02	DATE	A3

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			



CONCRETE ISLAND WITH ELEVATED CUT THROUGH



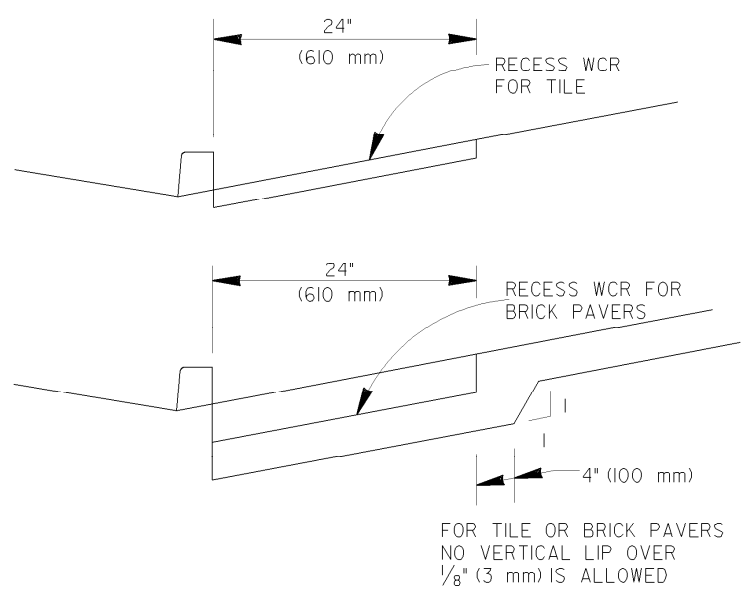
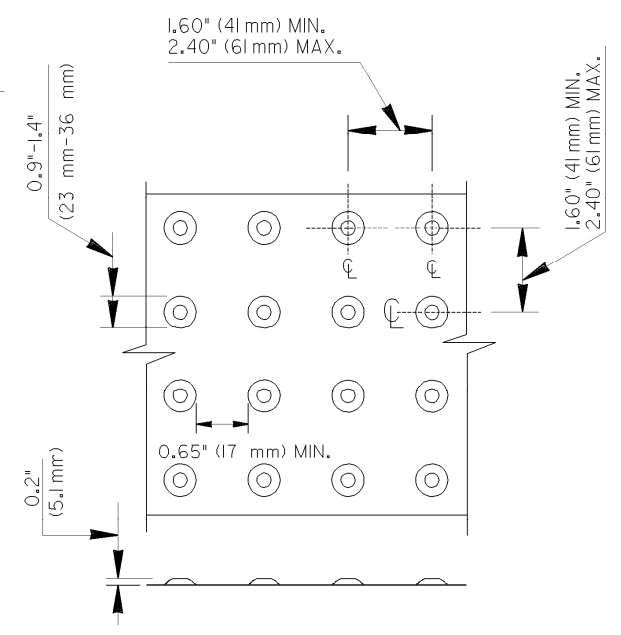
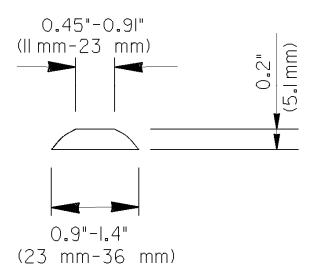
DETAIL FOR DETECTABLE WARNING AT CUT-THRU CONCRETE ISLAND

SIZE: DETECTABLE WARNINGS SHALL BE 24 INCHES (610 mm) IN THE DIRECTION OF PEDESTRAIN TRAVEL AND EXTEND THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE.

LOCATION: THE DETECTABLE WARNING SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE OR OTHER POTENTIAL HAZARD IS 6 TO 8 INCHES (150 mm TO 180mm) FROM THE CURB LINE OR OTHER POTENTIAL HAZARD, SUCH AS A REFLECTIVE POOL EDGE OR THE DYNAMIC ENVELOPE OF RAIL OPERATIONS.

DOMES SIZE AND SPACING: TRUNCATED DOMES SHALL HAVE A BASE DIAMETER OF 0.9 INCH TO 1.4 INCH (23mm-36mm) AT THE BOTTOM, A DIAMETER OF 0.45 INCH TO 0.91 INCH (11mm-23mm) AT THE TOP, THE TOP DIAMETER SHALL BE A MINIMUM OF 50% AND A MAXIMUM OF 65% OF THE BASE DIAMETER, A HEIGHT OF 0.2 INCH (5.1mm) AND A CENTER-TO-CENTER SPACING OF 2.40 INCHES (61mm) DESIRABLE 1.60 INCHES (41mm) MINIMUM MEASURED ALONG ONE SIDE OF A SQUARE ARRANGEMENT. DOMES SHALL HAVE A SQUARE ARRANGEMENT. DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DOMES.

VISUAL CONTRAST: DETECTABLE WARNING SURFACES SHALL CONTRAST VISUALLY WITH THE ADJACENT WALKING SURFACE EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT. THE MATERIAL USED TO PROVIDE VISUAL CONTRAST SHALL BE AN INTEGRAL PART OF THE DETECTABLE WARNING SURFACE.



MATERIALS:

NEW CONSTRUCTION
THE DETECTABLE WARNINGS SHALL BE MADE OF MATERIALS SPECIFIED ON OPL 87.

RETROFIT OF EXISTING RAMPS
SURFACED APPLIED MATERIALS WILL ONLY BE APPROVED TO BE USED ON EXISTING WHEELCHAIR RAMPS.

INSTALLATION:
BRICK PAVERS SHALL BE SET IN A WET MORTAR BED. THE BED SHALL BE PLACED ON CONCRETE. THE CONCRETE SHALL BE A MINIMUM OF 4" THICK.

CERAMIC TILE SHALL BE EPOXIED IN PLACE OR SET IN A WET MORTAR BED. MANUFACTURER RECOMMEND ADHESIVE OR FASTENER SHALL BE USED IN THE INSTALLATION.

ALL OTHER MATERIALS SHALL BE INSTALLED ACCORDING TO MANUFACTURES DETAILS OR INSTRUCTION.

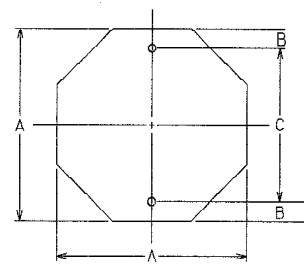
GENERAL NOTES:

- RETROFIT SURFACED APPLIED MATERIALS ONLY:
1. CHANGES IN LEVEL OF 1/4" (6.4 mm) HIGH MAXIMUM SHALL BE PERMITTED VERTICALLY ON SURFACED APPLIED MATERIALS.
 2. CHANGES IN LEVEL BETWEEN 1/4" (6.4 mm) HIGH MINIMUM AND 1/2" (13mm) HIGH MAXIMUM SHALL BE BEVELED WITH A SLOPE NOT STEEPER THAN 2:1.

NO SEPARATE PAYMENT WILL BE MADE FOR THE DETECTABLE WARNINGS. THE COST SHALL BE INCLUDED IN THE PRICE BID FOR SIDEWALK (OR CURB CUT RAMP IF THE ITEM IS INCLUDED IN THE PROPOSAL).

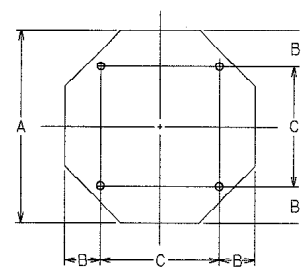
FOR CUT-THRU ISLANDS AND EXISTING RAMPS, WHERE NO SIDEWALK OR CURB CUT RAMPS ARE IN THE PROPOSAL. THE COST OF THE DETECTABLE WARINGS SHALL BE INCLUDED IN THE OVERALL BID PRICE SUBMITTED.

6-18-09		DEPARTMENT OF TRANSPORTATION	
ADDED RETROFIT DETAIL AND ADDED ALT. RAMP DETAIL AND GEN. NOTES		STATE OF GEORGIA	
ADDED TOLERANCE TO DTL.		SPECIAL DETAIL	
REVISED TRUNCATED DOMES		DETECTABLE WARNING SURFACE	
AND NOTES.		TRUNCATED DOME SIZE, SPACING	
REVISED		AND ALIGNMENT REQUIREMENTS	
REVISION		NO SCALE	
DATE		MARCH 12, 2002	
BY		NUMBER	
		A4	

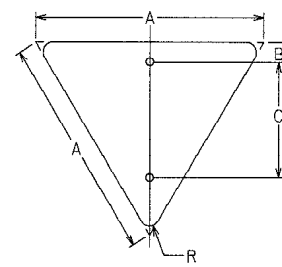


OCTAGON

A	B	C
24	3	18
36	3	30

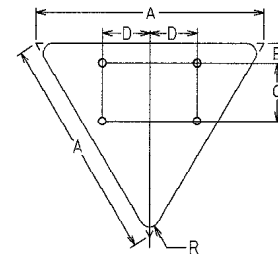


A	B	C
48	9	30

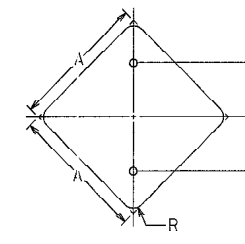


EQUILATERAL TRIANGLE

A	B	C	R
30	3	18	1 1/2
48	3	27	3

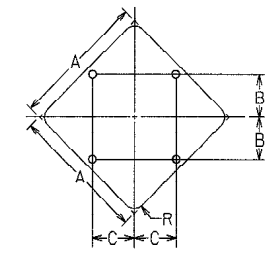


A	B	C	D	R
60	3	18	15	3



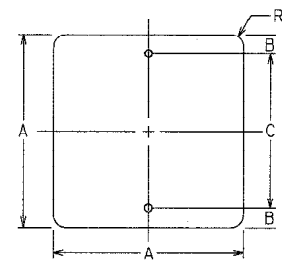
A	B	R
24	12	1 1/2
36	18	2 1/4

DIAMOND



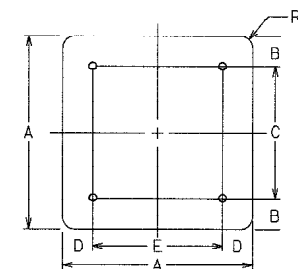
A	B	C	R
36	10	10	2 1/4
60	18	18	3 3/4

* FOR TWO POST ERECTION

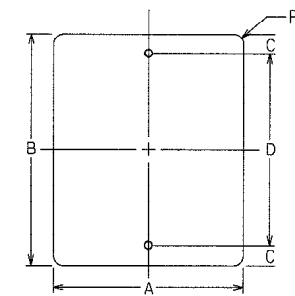


SQUARE

A	B	C	R
18	3	12	1 1/2
24	3	18	1 1/2
30	3	24	1 7/8

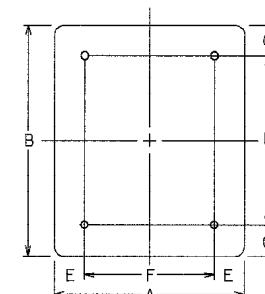


A	B	C	D	E	R
36	6	24	6	24	2 1/4
48	6	36	6	36	3

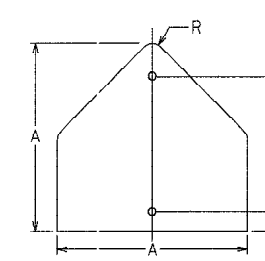


VERTICAL RECTANGLE

A	B	C	D	R
12	18	1 1/2	15	1 1/2
18	24	3	18	1 1/2
24	30	3	24	1 1/2
30	36	3	30	1 7/8

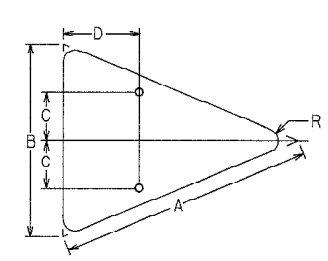


A	B	C	D	E	F	R
36	48	6	36	6	24	2 1/4
48	60	6	48	9	30	3



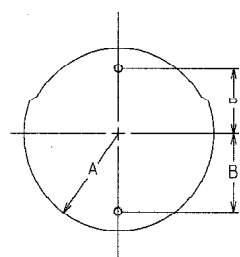
PENTAGON

A	B	C	R
30	21	3	1 7/8
36	24	3	2 1/4



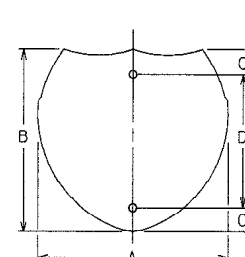
ISOSCELES TRIANGLE

A	B	C	D	R
40	30	7 1/2	12	1 7/8
48	36	9	15	2 1/4



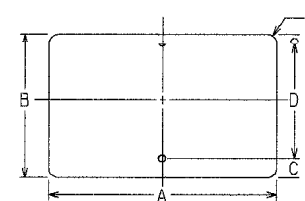
CIRCLE

A	B
15	12
18	15



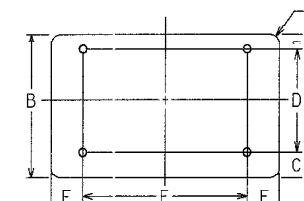
INTERSTATE SHIELD

A	B	C	D
24	24	3	10
30	24	3	18
36	36	6	24
45	36	6	24



HORIZONTAL RECTANGLE

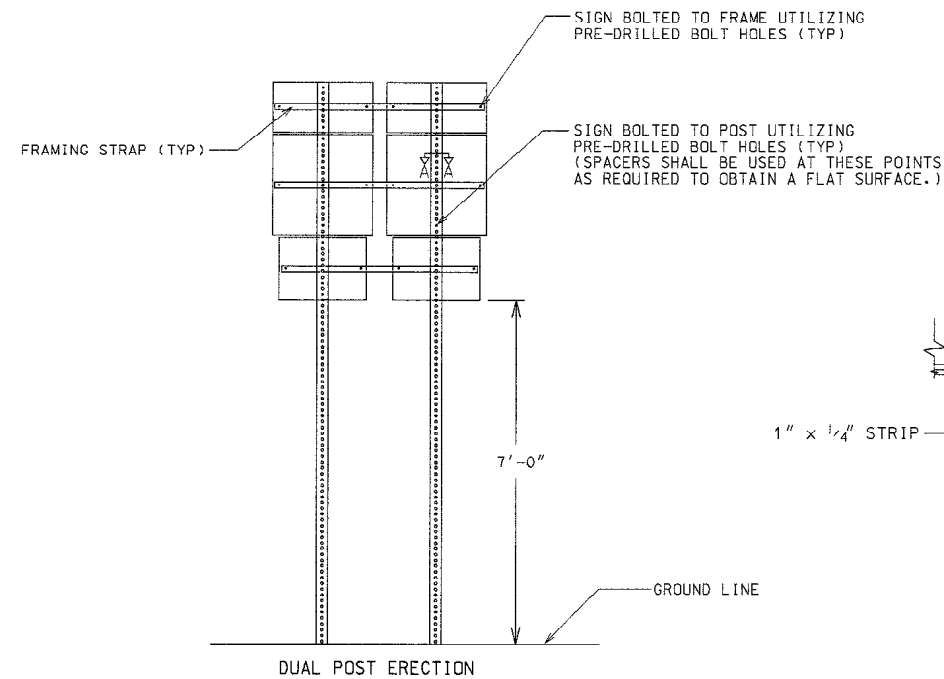
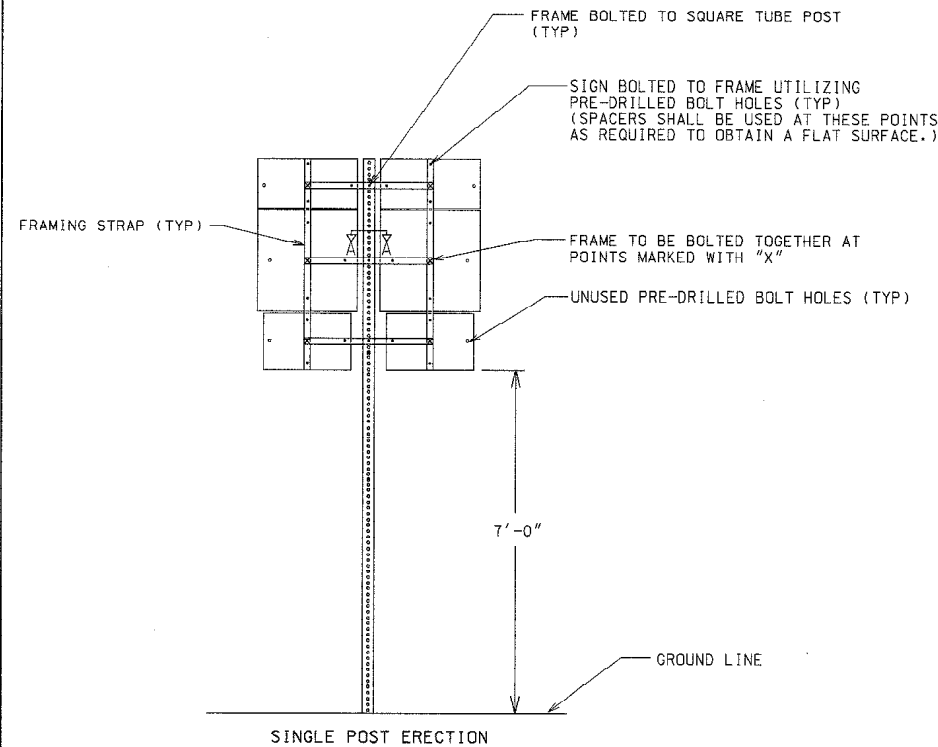
A	B	C	D	R
21	15	1 1/2	12	1 1/2
24	12	1 1/2	9	1 1/2
24	18	3	12	1 1/2
30	15	1 1/2	12	1 1/2
30	24	3	18	1 1/2
36	12	1 1/2	9	1 1/2
36	24	3	18	1 1/2
48	12	1 1/2	9	1 1/2
48	24	3	18	1 7/8



A	B	C	D	E	F	R
48	36	6	24	9	30	2 1/4
60	24	3	18	12	36	1 1/2
60	36	6	24	12	36	2 1/4

DATE	REVISIONS	GEORGIA DEPARTMENT OF TRANSPORTATION OFFICE OF TRAFFIC SAFETY & DESIGN
		DETAILS OF SIGN PLATES
		NO SCALE JANUARY 2000

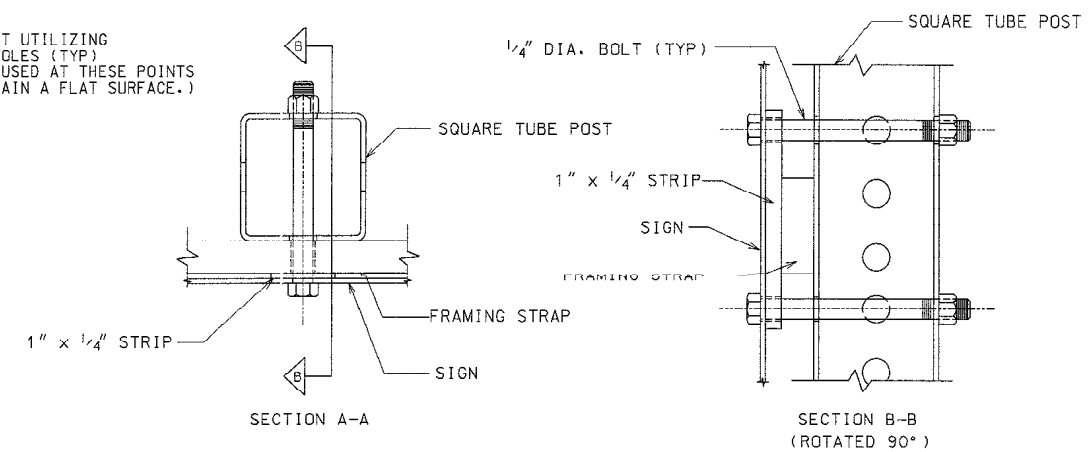
STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			



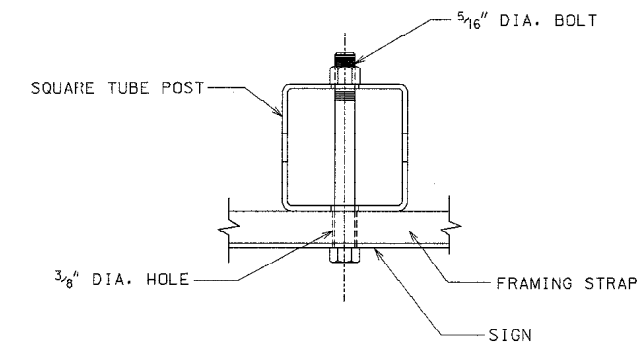
TYPICAL ASSEMBLY UNIT (BACK VIEW)

GENERAL NOTES:

1. STYLE OF FRAMING IS OPTIONAL. ALTERNATE DESIGNS ARE ACCEPTABLE UPON APPROVAL OF THE ENGINEER. FRAME SHALL BE DESIGNED SO AS TO HOLD THE ASSEMBLY IN A FIXED, RIGID POSITION.
2. FRAMING STRAPS SHALL BE GALVANIZED STEEL OR ALUMINUM.
3. STEEL SHALL BE A.S.T.M. DESIGNATION A-283, GRADE D, GALVANIZED IN ACCORDANCE WITH A.S.T.M. DESIGNATION A-123.
4. ALUMINUM SHALL BE ALLOY 6061-T6.
5. BOLTS, NUTS, WASHERS, AND SPACERS SHALL CONFORM TO THE STANDARD SPECIFICATIONS AND/OR SPECIAL PROVISIONS.
6. FRAMING STRAPS ON A DUAL POST ERECTION SHALL NOT BE BOLTED TO THE POST.



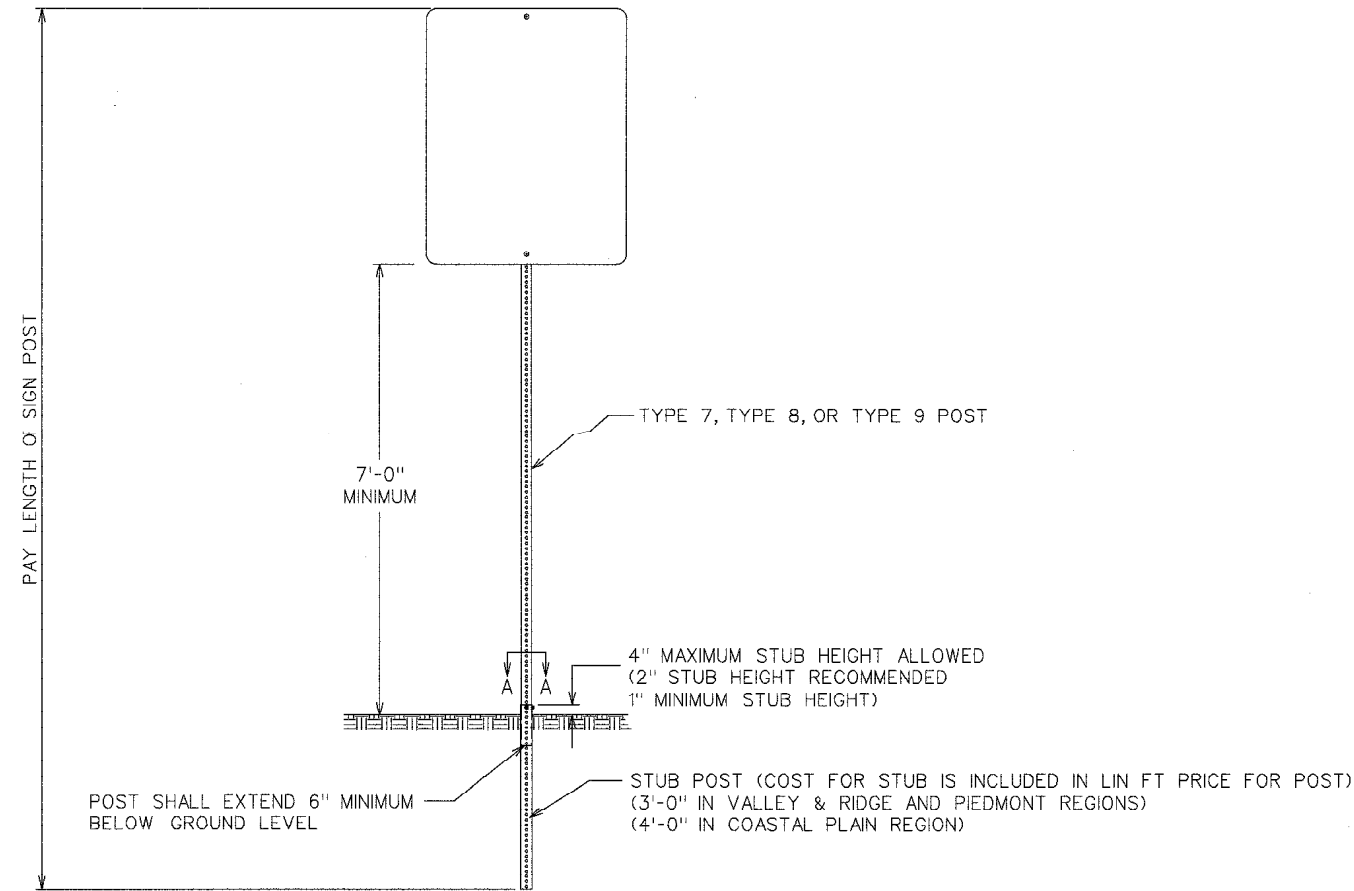
OPTION #1 - FRAMING STRAP WITHOUT MOUNTING HOLE
(ALL FRAMING STRAPS SHALL BE 1 1/2" x 1/2" x REQUIRED LENGTH)



OPTION #2 - FRAMING STRAP WITH MOUNTING HOLE
(ALL FRAMING STRAPS SHALL BE 2" x 1/2" x REQUIRED LENGTH)

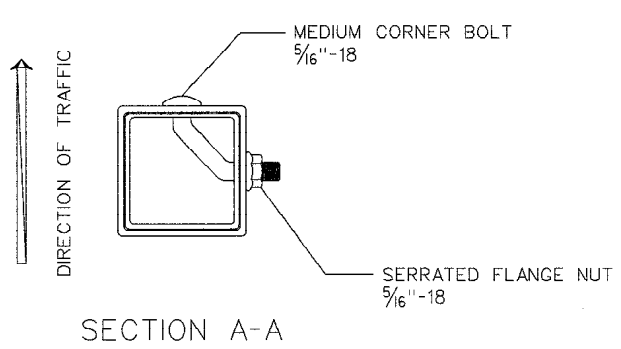
DATE	REVISIONS	GEORGIA DEPARTMENT OF TRANSPORTATION OFFICE OF TRAFFIC OPERATIONS
3/31/00	CHANGED U-CHANNEL POST TO SQUARE TUBE POST	
		DETAILS FOR TYPICAL FRAMING
		NO SCALE
		JANUARY 2000

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			



FRONT VIEW

POST	STUB SIZE
TYPE 7	2 1/4" x 2 1/4"
TYPE 8	2 3/4" x 2 3/4"
TYPE 9	2 1/2" x 2 1/2"



SECTION A-A

SIGN POST SELECTION CHART

70 MPH Wind Load Chart + 15% Gust Factor

Sign Centroid	SLIP BASE NOT REQUIRED				GROUND MOUNTED BREAKAWAY SIGN SUPPORT REQUIRED				
	TYPE 7 2" 1/4 ga.		TYPE 9 2-1/4" 1/4 ga.		TYPE 8 2-1/2" 1/2 ga.		TYPE 8 w / TYPE 9 Insert* 2-1/2" 1/2 ga. W / 2-1/4" 1/4 ga.		
	1 Post	2 Post	1 Post	1 Post	2 Post	3 Post	1 Post	2 Post	3 Post
	SQUARE FOOTAGE				SQUARE FOOTAGE				
6'	13.50	27.00	19.25	30.00	60.00	90.00	49.25	98.50	147.75
7'	11.60	23.20	16.50	25.75	51.50	77.25	42.25	84.50	126.75
8'	10.15	20.30	14.45	22.55	45.10	67.65	37.00	74.00	111.00
9'	9.00	18.00	12.85	20.00	40.00	60.00	32.85	65.70	98.55
10'	8.10	16.20	11.55	18.00	36.00	54.00	29.55	59.10	88.65
11'	7.40	14.80	10.50	16.40	32.80	49.20	26.90	53.80	80.70
12'	6.80	13.60	9.65	15.00	30.00	45.00	24.65	49.30	73.95
13'	6.25	12.50	8.90	13.85	27.70	41.55	22.75	45.50	68.25
14'	5.80	11.60	8.25	12.90	25.80	38.70	21.15	42.30	63.45
15'	5.00	10.00	6.45	10.10	20.20	30.30	16.55	33.10	49.65
16'	4.70	9.40	6.05	9.45	18.90	28.35	15.50	31.00	46.50
17'	4.40	8.80	5.70	8.90	17.80	26.70	14.60	29.20	43.80
18'	4.15	8.30	5.40	8.40	16.80	25.20	13.80	27.60	41.40
19'	3.95	7.90	5.10	7.95	15.90	23.85	13.05	26.10	39.15
20'	3.75	7.50	4.85	7.55	15.10	22.65	12.40	24.80	37.20

SIGN CENTROID IS DISTANCE FROM GROUND LEVEL TO BOTTOM OF SIGN PLUS HALF THE HEIGHT OF SIGN.
 EXAMPLE: 24" X 48" SIGN THAT IS 7 FEET FROM GROUND TO BOTTOM OF SIGN. ADD HALF OF 48" (24" OR 2 FT) PLUS 7 FT. = 9' CENTROID.

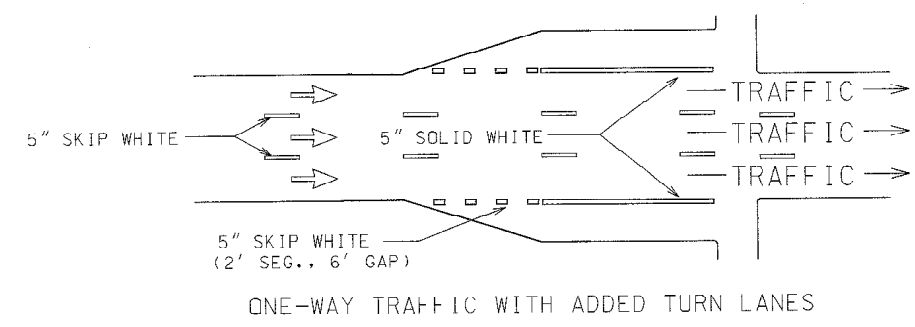
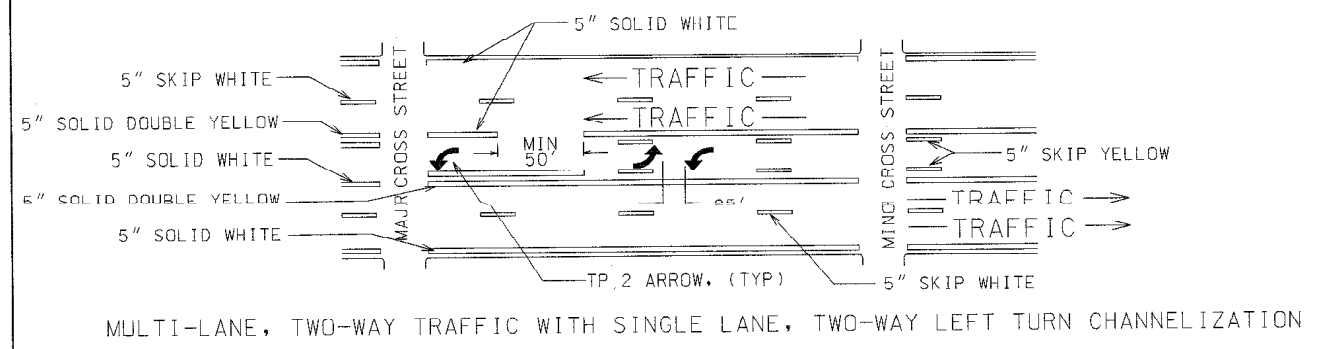
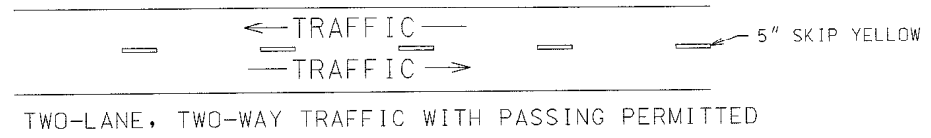
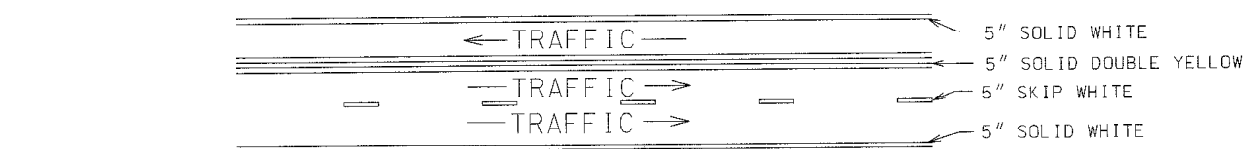
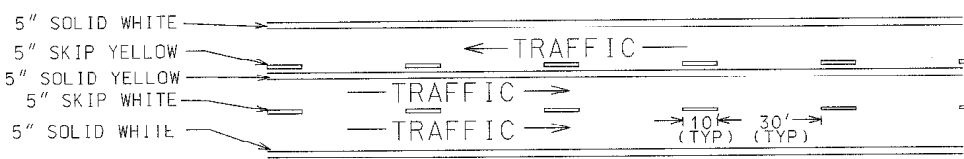
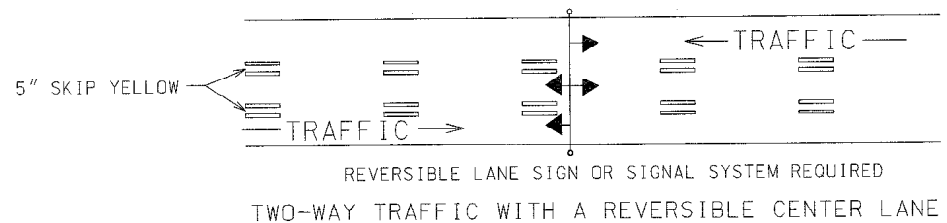
SIGN PLATE SHALL NOT EXCEED 48" IN WIDTH ON A SINGLE POST.

* TYPE 9 INSERT SHALL BE A CONTINUOUS POST INSERTED INTO THE TYPE 8 POST WHERE REQUIRED. THE INSERT POST SHALL EXTEND FROM THE BOTTOM OF THE SLIP BASE UPPER ASSEMBLY TO 4" BELOW THE BOTTOM OF THE SIGN. THE INSERT POST SHALL NOT EXTEND ABOVE THE BOTTOM OF THE SIGN. PAYMENT FOR THE INSERT POST SHALL BE PER LINEAR FOOT OF TYPE 9 POST.

GROUND MOUNTED BREAKAWAY SIGN SUPPORT WILL BE MEASURED AND PAID FOR SEPARATELY. THE COST FOR THIS WORK SHALL INCLUDE THE UPPER AND LOWER ASSEMBLY, STUB POST, CLASS "A" CONCRETE, ALL HARDWARE NECESSARY TO COMPLETE THE INSTALLATION, AND BE INCLUDED IN THE BID PRICE SUBMITTED FOR ITEM 636-3010.

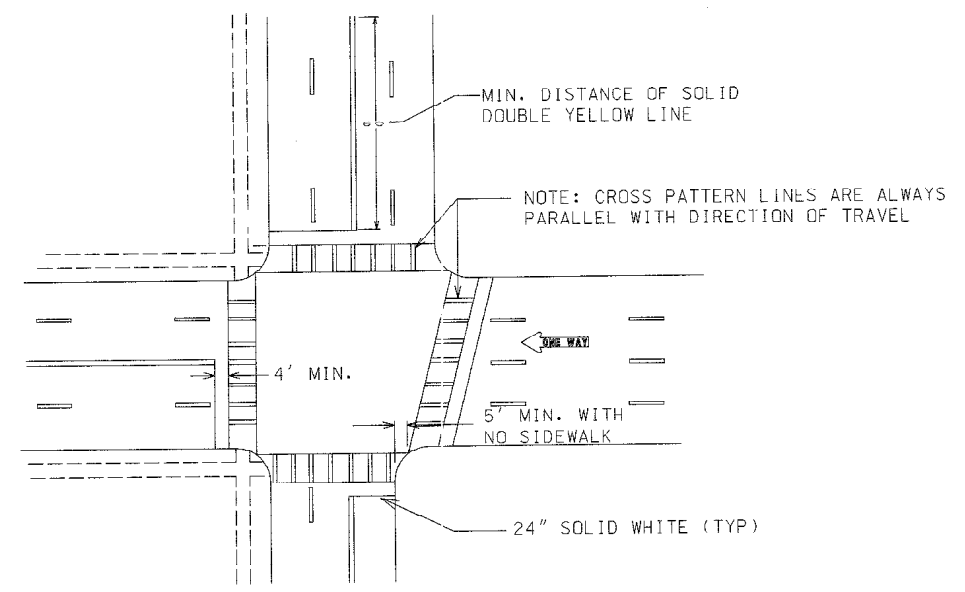
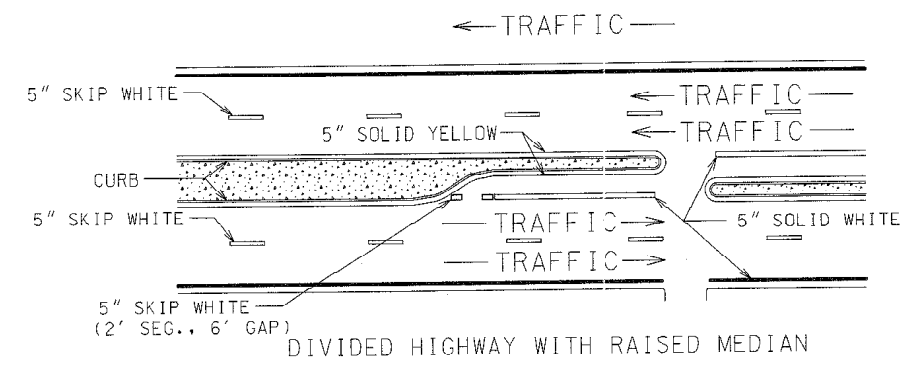
DATE	REVISIONS	GEORGIA DEPARTMENT OF TRANSPORTATION OFFICE OF TRAFFIC SAFETY & DESIGN
		TYPE 7, 8, AND 9 SQUARE TUBE POST INSTALLATION DETAIL
		NO SCALE
		JULY 2002

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			

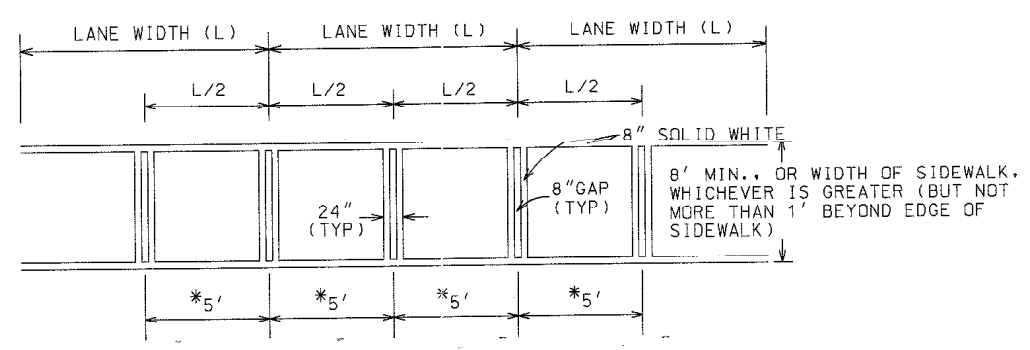


GENERAL NOTES:

1. SPACING BETWEEN DOUBLE LINES SHALL BE EQUAL TO THE LINE WIDTH.
2. EDGE LINES SHALL BE PLACED A MINIMUM OF 4 INCHES FROM THE NORMAL EDGE OF PAVEMENT.



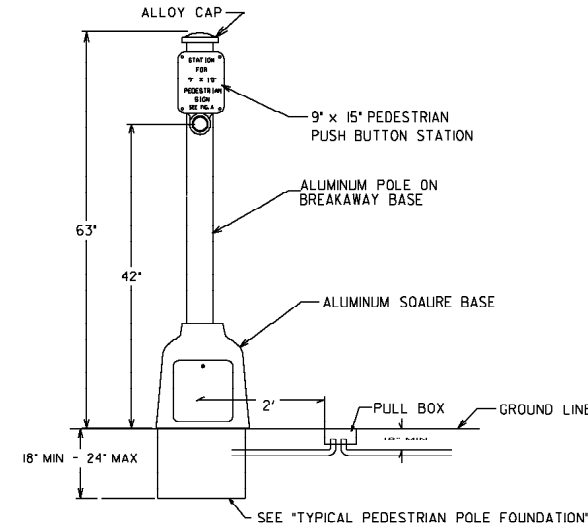
TYPICAL LOCATION OF CROSSWALKS AND STOP BARS



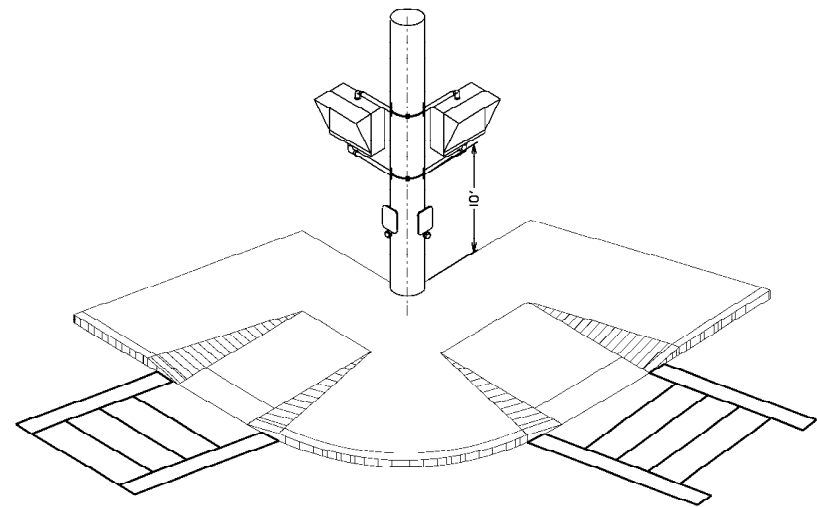
*USE WHERE THE LANE WIDTH EXCEEDS 12' OR WHERE LANE LINES HAVE BEEN OMITTED

CROSSWALK DETAIL

DATE	REVISIONS	GEORGIA DEPARTMENT OF TRANSPORTATION OFFICE OF TRAFFIC SAFETY & DESIGN
		DETAILS OF PAVEMENT MARKING PLACEMENT NON-LIMITED ACCESS ROADWAY
		NO SCALE
		JANUARY 2000

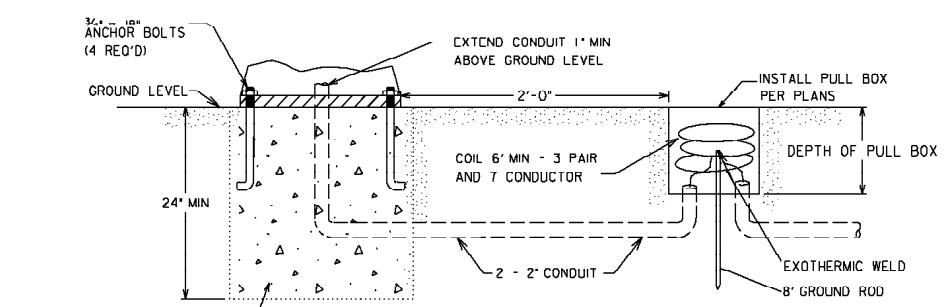
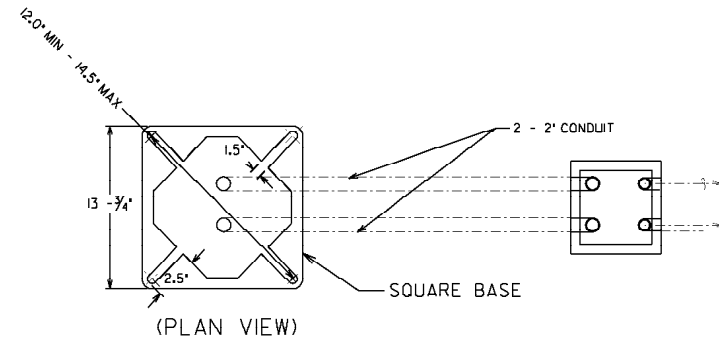


PEDESTRIAN PUSH BUTTON POST



PEDESTRIAN SIGNAL HEAD ORIENTATION FOR SIDE OF POLE MOUNTING

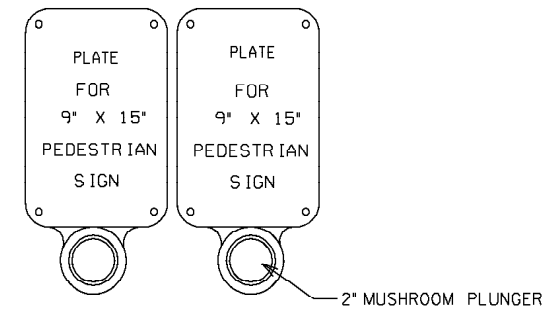
INSTALL PEDESTRIAN SIGNAL HEADS SO THAT VEHICLES MAKING TURNS WILL NOT DAMAGE THE EQUIPMENT



TYPICAL PEDESTRIAN POLE FOUNDATION

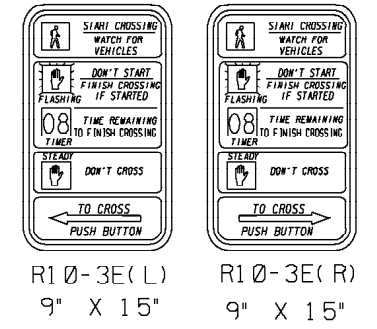
CLASS A CONCRETE PER SECTION 500 OF STANDARD SPECIFICATIONS

Guidelines For Usage On Metric Projects
 When these details are incorporated into plans and or projects that are being prepared or constructed in metric units, exact or precise conversion to metric units is not required. The dimensions shown that are in feet and inches may be converted to corresponding metric units using the following "Rounded-Off" conversion factors: 1" = 25mm, 4" = 100mm, and 12" or 1' = 300mm. All measurement notes that refer to linear feet and square yards shall be interpreted to mean linear meters and square meters.

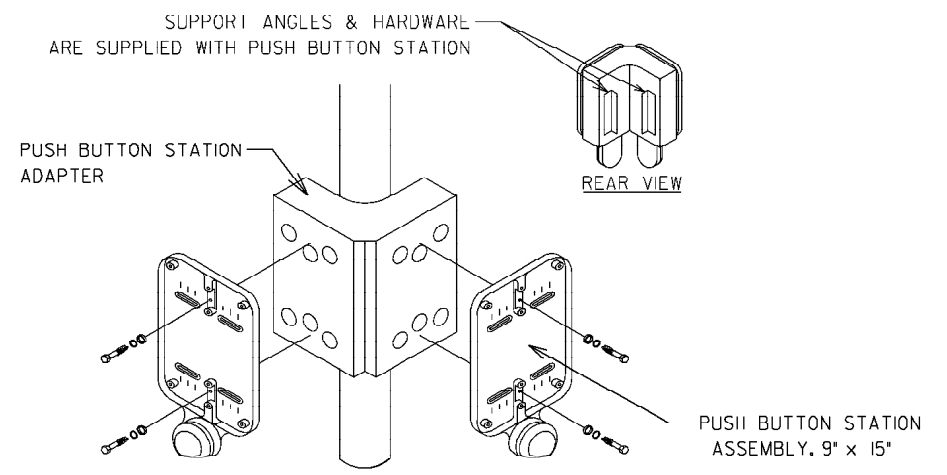


A "PUSH BUTTON STATION" IS THAT PIECE OF EQUIPMENT THAT CONTAINS THE PEDESTRIAN INSTRUCTIONS SIGN PLATE AND THE PUSH BUTTON

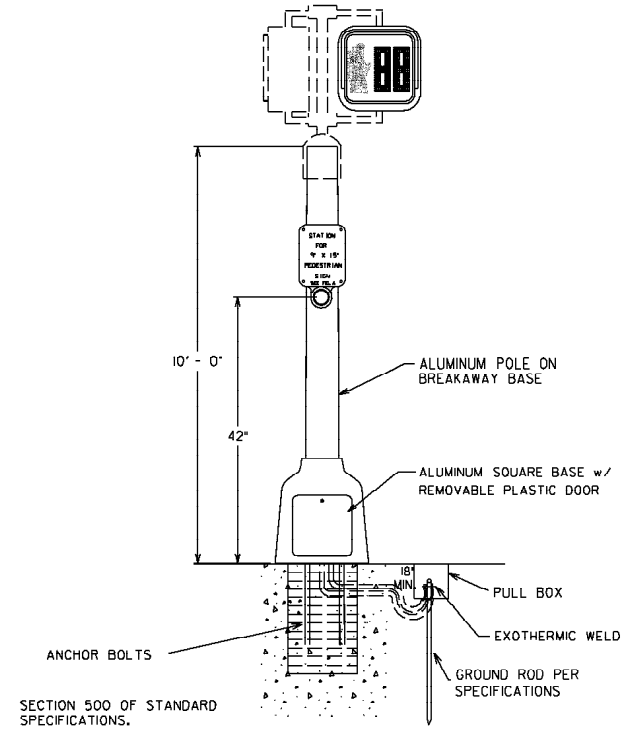
PEDESTRIAN PUSH BUTTON STATION



PEDESTRIAN SIGNS



DOUBLE PUSH BUTTON STATION ADAPTER FOR 4" DIA. PEDESTRIAN POLE



DETAIL FOR PEDESTRIAN SIGNAL POLES

NOTE: DETAILS SHOWN IS FOR TOP POST MOUNTING ASSEMBLY ON 10 FEET PEDESTRIAN POLE. A CLAMSHELL MOUNTING ASSEMBLY (NOT SHOWN) MAY BE USED AS APPROVED BY THE DEPARTMENT. THE CLAMSHELL MOUNTING HARDWARE ASSEMBLY SHALL MEET THE SAME GOOT STANDARDS AS THE PEDESTRIAN SIGNAL HOUSING IN PAINT AND MATERIAL.

DATE	DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA
REVISION DESCRIPTION	TRAFFIC SIGNAL DETAIL PEDESTRIAN FACILITIES INSTALLATION DETAILS
REV. BY:	DETAIL NUMBER APRIL 2010 NOT TO SCALE REPORT ERRORS TS-03A